

# CITY PLAN COMMISSION STAFF REPORT

City of Kansas City, Missouri
City Planning & Development Department
<a href="https://www.kcmo.gov/cpc">www.kcmo.gov/cpc</a>

August 15, 2023

# Project Name 1901 Locust Townhomes

### Docket #3

# Request

CD-CPC-2023-00096 Development Plan

# **Applicant**

Adam DeGonia McClure

# Owner

Lance Carlton UC-B Homebuilders, LLC

Location 1901 Locust Street Area About 0.47 acres

Zoning M1-5
Council District 4<sup>th</sup>
County Jackson
School District KCMO 110

# **Surrounding Land Uses**

North: Paved Parking, zoned M1-5
South: Residential, zoned M1-5
East: Vacant, zoned M1-5
West: Residential, zoned M1-5

# **Major Street Plan**

Locust Street is not identified on the City's Major Street Plan.

### Land Use Plan

The Greater Downtown Area Plan recommends Downtown Mixed Use for this location.

# **APPROVAL PROCESS**

Staff
Review
City Plan
Commission
City Council

## **PROJECT TIMELINE**

The application for the subject request was filed on 06/10/2023. Scheduling deviations from 2023 Cycle N have occurred.

- The applicant needed additional time to work with the Water and Fire Departments.

# **NEIGHBORHOOD AND CIVIC ORGANIZATIONS NOTIFIED**

The subject site is located within the Crossroads Community Association.

### REQUIRED PUBLIC ENGAGEMENT

Public engagement as required by 88-505-12 does apply to this request. Applicant hosted a meeting on July 24, 2023. A summary of the meeting is attached to the staff report, see Attachment #3.

# **EXISTING CONDITIONS**

The subject property is currently vacant.

# **SUMMARY OF REQUEST + KEY POINTS**

The applicant is seeking approval of a residential development plan for 13 units (townhomes) with a drive aisle.

# **CONTROLLING + RELATED CASES**

Ordinance No. 950083 – Vacating Locust Street from the south line of 19<sup>th</sup> street to the north line of 20<sup>th</sup> street, approved 02/19/1995.

# PROFESSIONAL STAFF RECOMMENDATION

Docket #3 Approval with Conditions

# **PLAN REVIEW**

The proposed residential development is for 13 for-sale townhome units located in two buildings. Each unit has a garage that can accommodate two vehicles totaling 26 parking spaces. Each unit can be accessed from the main drive or via a sidewalk along Locust Street and a trail along the eastern side of the property.

The development is proposing interior bicycle parking, a cluster mailbox, and individual garbage cans for each unit. Access to the site is off 19<sup>th</sup> street, the location of the development is ideal for future residents who wish to ride their bike due to the proximity of bike lanes on 19<sup>th</sup> Street.

The developer is proposing landscaping in compliance with the Zoning and Development Code. The developer is also adding a trail along the eastern side of the property that will provide residents in the eastern structure access to the individual units from 19<sup>th</sup> street. The trail terminates at the drive access lane in between both structures.

Proposed building materials consist of stucco, red brick, dark red brick, metal, and glazing. While brick is a high-quality construction material, there is a large amount of stucco proposed throughout the project. The overall design of both structures needs improvement. The northern façade facing 19th Street is mostly split between a blank wall of brick for the first two floors and blank stucco for the top floor, the interior and southern facades of the development consist primarily of blank stucco. Staff is proposing a condition that the architectural details and construction materials be revised prior to ordinance request.

# **PLAN ANALYSIS**

\*indicates adjustment/deviation

Standards	Applicability	Meets	More Information
Lot and Building Standards (88-140)	Yes	Yes	
Parkland Dedication (88-408)	Yes	Yes, subject to conditions	
Parking and Loading	Yes	Yes	
Standards (88-420)			
Landscape and Screening Standards (88-425)	Yes	Yes, subject to conditions	
Outdoor Lighting Standards (88-430)	Yes	Yes, subject to conditions	
Sign Standards (88-445)	Yes	Yes, subject to conditions	
Pedestrian Standards (88-450)	Yes	Yes	

# **SPECIFIC REVIEW CRITERIA**

Development Plans, Project Plan, or Site Plan (88-516-05)

In order to be approved, a development plan, project plan, or site plan must comply with all of the following criteria:

# A. The plan must comply with all standards of the Zoning and Development Code and all other applicable City ordinances and policies;

The plan shall comply with all standards of the Zoning and Development Code and all other applicable City ordinances and policies when the conditions of approval have been met.

The developer is working with the Fire Department to ensure the structures meet the requirements for aerial access to all the units to guarantee the safety of the residents. The proposed development has a maximum height of 43' (feet) for the buildings closest to 19th Street and 29' (feet) and 11.5" (inches) for the two southern units on the southern edge of the eastern building. The developer is still working with the Fire Department regarding the railing for the rooftop deck for the southernmost unit. The corrections to the plan will not change the orientation, unit count, or create a large enough change in the development plan to require an amendment, therefore the Fire Department has agreed that the correction can be discussed and approved prior to building permit.

- B. The proposed use must be allowed in the district in which it is located;
  - The proposed use is allowed in an M1-5 zoning district. Since there are 2 buildings proposed for the property a development plan is required.
- C. Vehicular ingress and egress to and from the site, and circulation within the site must provide for safe, efficient, and convenient movement of traffic not only within the site but on adjacent roadways;

  Vehicular access to the site is off 19<sup>th</sup> Street. The internal circulation will lead directly to the privately-owned garages. The circulation is efficient and convenient within the site and onto adjacent roadways.
- D. The plan must provide for safe, efficient, and convenient non-motorized travel opportunities, being pedestrian and bicycle movement, on the subject site;

The plan provides safe, efficient, and convenient pedestrian movement by providing a trail on the east side of the property providing residents living in the east building a path from 19<sup>th</sup> Street south to the edge of the property. The developer is proposing short and long term bicycle parking within each unit.

E. The plan must provide for adequate utilities based on City standards for the particular development proposed.

The plan provides adequate utilities based on City standards.

F. The location, orientation, and architectural features, including design and material, of buildings and other structures on the site must be designed to be compatible with adjacent properties.

The proposed development is in an area of the city that needs increased diversity of housing choices. The western structure is facing Locust Street and includes street level entrances to each unit. The eastern lot also includes street level entrances to each unit while facing the vacant lot to the east. The construction materials consist of stucco, red brick, dark red brick, metal, and glazing.

Brick is a high quality construction material, however the amount of stucco and the overall design of both structures needs improvement. The southern elevations of both buildings show a large amount of stucco directly facing an existing residential structure, more windows, architectural detailing, and architectural variation need to be provided. Nineteenth street is a prominent street in the Crossroads

area, so the architecture will need to more closely match the development to the West to create a more cohesive streetscape. The northern elevations of both structures also show a large amount of stucco facing 19th Street and the architectural design will need to include additional windows or a façade that mimics the entryway to a building. Additionally, the interior of the development needs to create a more welcoming environment for the residents. The interior facing walls of the development should more closely match the exterior facing walls with the added material changes, a reduction in the amount of stucco is also requested by staff.

G. Landscaping, berms, fences, and/or walls must be provided to buffer the site from undesirable views, noises, lighting, or other off-site negative influences and to buffer adjacent properties from negative influences that may be created by the plan.

The developer has proposed landscaping in compliance with the Zoning and Development Code. There are four trees along 19th Street and Fescue Sod along the outside of both buildings. Locust Street was vacated in 1995 and is currently a private street. The landscaping along Locust street has been in place for a few years and will remain. The developer is providing an additional tree on the southeast corner in lieu of an additional tree on Locust Street.

H. The design of streets, drives, and parking areas within the project should result in a minimum of area devoted to asphalt or other impervious surfaces consistent with the needs of the project and city code requirements.

The subject property is currently paved. The proposed use and design of streets, drives, and parking areas within the project will be slightly less impervious than what is currently on the property. The developer is proposing a trail surrounded by fescue sod on the east side of the proposed project.

 The plan must identify trees to be removed and trees to be preserved during the development of the subject property with the goal of saving trees that are not required to be removed for the development of the property.

There are no trees on the property. The applicant has provided landscape plans showing the existing trees along Locust Street and the proposed trees for the rest of the subject property.

### **ATTACHMENTS**

- 1. Conditions Report
- 2. Applicants Submittal
- 3. Public Engagement Materials

# PROFESSIONAL STAFF RECOMMENDATION

City staff recommends **Approval with Conditions** as stated in the conditions report.

Respectfully Submitted,

Larisa Chambi, AICP

Lead Planner

# KANSAS CITY Planning & Dev

# **Plan Conditions**

Report Date: August 10, 2023

Case Number: CD-CPC-2023-00096 Project: 1901 Locust Townhomes

Condition(s) by City Planning and Development Department. Contact Larisa Chambi at (816) 513-8822 / Larisa.Chambi@kcmo.org with questions.

1. The developer shall screen all roof and/or ground mounted mechanical and utility equipment in compliance with 88-425-08.

- 2. The developer shall submit an affidavit, completed by a landscape architect licensed in the State of Missouri, verifying that street trees have been installed in accordance with the approved street tree planting plan and are healthy prior to Certificate of Occupancy.
- 3. The developer shall submit an affidavit, completed by a landscape architect licensed in the State of Missouri, verifying that all landscaping required of the approved plan has been installed in accordance with the plan and is healthy prior to Certificate of Occupancy.
- 4. The developer shall submit an affidavit, prepared by an engineer licensed in the State of Missouri, verifying that all outdoor lighting has been installed in accordance with approved plans and that lighting levels do not exceed that shown on the approved lighting plan at the property lines prior to Certificate of Occupancy.
- 5. Signage was not reviewed with this submittal, all signage shall conform to 88-445 and shall require a sign permit prior to installation.
- 6. The proposed development has too many blank walls throughout the project, the developer shall submit revised elevations showing an increase in architectural fenestration, variety in construction materials and/or proposed locations for murals on the northern, southern, and interior facades of both buildings to better match the surrounding area prior to ordinance request.
- 7. The developer shall revise the landscaping plan to include additional native landscaping materials and confirm the proposed ground cover is compliant with Section 88-425-10-D of the Zoning and Development Code prior to ordinance request.

Condition(s) by City Planning and Development Department. Contact Terry Thomas at (816) 513-2510 / Terry.A.Thomas@kcmo.org with questions.

- 8. The developer shall submit a letter to the Land Development Division from a Licensed Civil Engineer, Licensed Architect, or Licensed Landscape Architect, who is registered in the State of Missouri, that identifies sidewalks, curbs, and gutters in disrepair as defined by Public Works Department's "OUT OF REPAIR CRITERIA FOR SIDEWALK, DRIVEWAY AND CURB revised 11/5/2013" and based on compliance with Chapters 56 and 64 of the Code of Ordinances for the sidewalks, curbs, and gutters where said letter shall identify the quantity and location of sidewalks, curbs, gutters that need to be constructed, repaired, or reconstructed to remedy deficiencies and/or to remove existing approaches no longer needed by this project. The developer shall secure permits to repair or reconstruct the identified sidewalks, curbs, and gutters as necessary along all development street frontages as required by the Land Development Division and prior to issuance of any certificate of occupancy permits including temporary certificate occupancy permits.
- 9. The developer shall submit plans for grading, siltation, and erosion control to Land Development Division for review and acceptance, and secure a Site Disturbance permit for any proposed disturbance area equal to one acre or more prior to beginning any construction activities.
- 10. The developer shall submit a Storm Drainage analysis from a Missouri-licensed civil engineer to the Land Development Division evaluating proposed improvements and impact to drainage conditions. Since this project is within a "Combined Sewer Overflow" (CSO) district, the project shall be designed to retain rainfall of 1.5 inch depth over the entire site to simulate natural runoff conditions and reduce small storm discharge to the combined sewer system and manage the 10-year storm and 100-year storm per currently adopted APWA standards. The analysis shall be submitted, and the developer shall secure permits to construct any improvements required by the Land Development Division prior to recording the plat.

Condition(s) by City Planning and Development Department. Contact Terry Thomas at (816) 513-2510 / Terry.A.Thomas@kcmo.org with questions.

- 11. The developer shall obtain the executed and recorded City approved grading, temporary construction, drainage/sewer, or any other necessary easements from the abutting property owner(s) that may be required prior to submitting any public improvements crossing properties not controlled by the developer and include said document(s) within the public improvement applications submitted for permitting
- 12. The developer shall submit an analysis to verify adequate capacity of the existing sewer system as required by the Land Development Division prior to issuance of a building permit to connect private system to the public sewer main and depending on adequacy of the receiving system, make other improvements as may be required.
- 13. The developer shall grant a city approved pedestrian right-of-way easement, for the portions of the public sidewalks approved to be outside of the street right-of-way, to the City as required by the Land Development Division, prior to recording the plat.
- 14. The developer shall submit verification of vertical and horizontal sight distance for the drive connection to public right-of-way to the Land Development Division and make improvements to ensure local jurisdiction and/or minimum AASHTO adequate sight distance standards are met, prior to issuance of any certificate of occupancy.
- 15. The developer shall submit a streetscape plan for approval and permitting by the Land Development Division prior to beginning construction of the streetscape improvements in the public right of way, and construct ADA compliant ramps at all required locations where new private drives are being added, or where existing sidewalks are modified or repaired.
- 16. The developer shall submit covenants, conditions and restrictions to the Land Development Division for review by the Law Department for approval for the maintenance of private open space and enter into a covenant agreement for the maintenance of any stormwater detention area tracts, prior to recording the plat.
- 17. The developer shall cause the area to be platted and processed in accordance with Chapter 88, Code of Ordinances of the City of Kansas City, Missouri.

Condition(s) by Fire Department. Contact Michael Schroeder at (816) 513-4604 / michael.schroeder@kcmo.org with questions.

- 18. The project shall meet the fire flow requirements as set forth in Appendix B of the International Fire Code 2018. (IFC-2018 § 507.1)
- 19. Fire hydrants shall be installed and operable prior to the arrival of any combustible building materials onto the site. (IFC-2018 § 501.4 and 3312.1; NFPA 241-2013 § 8.7.2) Fire hydrant distribution shall follow IFC-2018 Table C102.1
- 20. IFC (2018) § D105.1 The railing for the roof deck on the South perimeter wall of the east structure shall be set back 6 feet from the eaves of all exterior walls, or an alternative method approved by the Fire Marshal.

Condition(s) by Water Services Department. Contact Heather Massey at (816) 513-2111 / heather.massey@kcmo.org with questions.

- 21. The developer shall ensure that water and fire service lines should meet current Water Services Department Rules and Regulations. Prior to C of O. https://www.kcwater.us/wp-content/uploads/2022/05/2022-Rules-and-Regulations-for-Water-Service-Lines-Final.pdf
- 22. No water service tap permits will be issued until the public water main is released for taps.
- 23. The existing Full flow fire meter and backflow shall be relocated to the West side of Locust on E 19th St.

Condition(s) by Water Services Department. Contact Lucas Kaspar at (816) 513-2131 / Lucas.Kaspar@kcmo.org with questions.

- 24. The developer shall submit a Storm Drainage analysis from a Missouri-licensed civil engineer to the Land Development Division evaluating proposed improvements and impact to drainage conditions. Since this project is within a "Combined Sewer Overflow" (CSO) district, the project shall be designed to retain rainfall of 1.5 inch depth over the disturbed area to simulate natural runoff conditions and reduce small storm discharge to the combined sewer system. Manage the 10-year storm and 100-year storm per currently adopted APWA standards. The analysis shall be submitted, and the developer shall secure permits to construct any improvements required by the Land Development Division prior to recording the plat.
- 25. The developer shall cause the area to be platted and processed in accordance with Chapter 88, Code of Ordinances of the City of Kansas City, Missouri, as amended, commonly known as the Development Regulations.
- 26. The existing public sanitary sewer line in the vacated alley must be upsized to a 12" main between manhole 366 and manhole 637 to accommodate the development.

Condition(s) by Water Services Department. Contact Lucas Kaspar at (816) 513-2131 / Lucas.Kaspar@kcmo.org with questions.

- 27. The developer must secure permits to extend sanitary and storm water conveyance systems to serve all proposed lots within the development and determine adequacy of receiving systems as required by the Land Development Division, prior to recording the plat or issuance of a building permit whichever occurs first.
- 28. The internal sanitary sewers shall be a private sewer main located within a private easement and covered by a Covenant to Maintain Private Sewers document acceptable to KC Water.
- 29. The developer shall provide a Covenant to Maintain Private Sewer acceptable to KC Water prior to recording the final plat or issuance of any building permits, whichever occurs first.
- 30. The developer must enter into a covenant agreement for the maintenance of any storm water detention area tracts as required by the Land Development Division, prior to recording the plat.
- 31. The developer must dedicate an additional 7.5 feet of right of way along the South side of E. 19th Street to accommodate the water service line from the previous 19th and Oak development into the public right of way.

# 19TH AND LOCUST TOWNHOMES **DEVELOPMENT PLAN**

**LOCATED IN** SECTION 8 TOWNSHIP 49N, RANGE 33W KANSAS CITY, JACKSON COUNTY, MISSOURI

		2022024222 242		
	ect#	2022001299-010		
a	ect Name:	19TH AND LOCUST		
	Existing	<b>Zoning</b> M1-5		
	Proposed	M1-5		
b	· · · · · · · · · · · · · · · · · · ·	ıl Land Area		
	Existing		Acres	
С	Rig	ht-of-Way	•	
	Existing	0	Acres	
	Proposed	0	Acres	
d	Net	Land Area		
	Existing		Acres	
	Proposed	0.47	Acres	
е	Proj	oosed Uses		
	-	WNHOME		
f		ht & Number of Floors		
	Number of floors		stories	BLDG 1
	Height Above Grade		feet	
	Number of floors Height Above Grade		stories feet	BLDG 2
	Number of floors		stories	
	Height Above Grade		feet	BLDG 3
	Number of floors		stories	
	Height Above Grade		feet	BLDG 4
	Number of floors	3	stories	DI DC F
	Height Above Grade	45	feet	BLDG 5
	Number of floors	3	stories	BLDG 6
	Height Above Grade	45	feet	DEDG 0
	Number of floors		stories	BLDG 7
	Height Above Grade		feet	
	Number of floors		stories	BLDG 8
	Height Above Grade		feet	
	Number of floors		stories	BLDG 9
	Height Above Grade Number of floors		feet stories	
	Height Above Grade		feet	BLDG 10
	Number of floors		stories	
	Height Above Grade		feet	BLDG 11
	Number of floors		stories	
	Height Above Grade		feet	BLDG 12
	Number of floors	3	stories	DLDC 12
	Height Above Grade	45	feet	BLDG 13
g	<b>Gross Floor Area &amp; Number of Ur</b>	nits		
	Building Footprint	830	SF	
	Gross Area Per Building	2,490	SF	
	Number of Buildings		EA	
1	Total Gross Area	32,370	SF	
h	Building Coverage & Floor Area R			
	Coverage (Total Footprint Area) FAR	32,370 1.58		
 i	Density	1.30	<u> </u>	
	Gross	68,732	SF per A	cre
	Net	68,732	•	
 j	Vehicle Parking	Garage Provided		
	Ratio Required	1 Dwelling per unit		
	Stalls Provided	26		
k	Bike Parking	Interior Spaces Provid	ded	
	Short-Term Ratio Required	0		
	Short-Term Stalls Provided	0		
	Lawa Tawa Datia Daawiya d	0		
	Long-Term Ratio Required		INITEDIO	ND.
	Long-Term Stalls Provided	26	INTERIC	, N
	Long-Term Stalls Provided Timeline			/K
 I	Long-Term Stalls Provided  Timeline Estimated Start Date:	10/1/2023		
l m	Long-Term Stalls Provided Timeline	10/1/2023 10/1/2025		<u> </u>



NOT TO SCALE **SECTIONS 8, TOWNSHIP 49N, RANGE 33W** 

<u>1-70</u>			
S			
I-670			
	I-670	I-70	
		PROJECT LOC	CATION
Google Earth			

KANSAS CITY, JACKSON COUNTY, MISSOURI

SH	EET LIST						
Sheet Number	Sheet Title						
C001	COVER SHEET						
C200	SITE PLAN						
C201	PRELIMINARY PLAT						
C300	UTILITY PLAN						
C301	PHOTOMETRIC PLAN						
C302	PHOTOMETRIC PLAN						
C400	GRADING PLAN						
L101	LANDSCAPE PLAN						

**FLOODPLAIN:** 

PER THE "FLOOD INSURANCE RATE MAP", COMMUNITY PANEL NO. 29095C0254G, HAVING AN EFFECTIVE JANUARY 20, 2017 AS PUBLISHED BY FEDERAL EMERGENCY MANAGEMENT AGENCY. OUR REVIEW OF THIS MAP INDICATES THAT THIS PARCEL OF LAND LIES WITHIN ZONE X WHICH IS "AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN."

LEGAL DESCRIPTION LOT 2, 19 AND OAK, A SUBDIVISION IN KANSAS CITY, JACKSON COUNTY, MISSOURI.

DEVELOPER:	
UC-B PROPERTIES	
CONTACT NAME:	LANCE CARLTON
ADDRESS:	4325 TROOST AVENUE
	KANSAS CITY, MO 64110
PHONE:	816.599.8651
EMAIL:	LCARLTON@UC-BPROPERTIES.COM
PLANNER & CIVIL ENGINE	ER
MCCLURE ENGINEERING	
CONTACT NAME:	ADAM DEGONIA
ADDRESS:	1700 SWIFT ST STE 100
	NORTH KANSAS CITY, MO 64116
PHONE:	816.502.3915
EMAIL:	ADEGONIA@MCCLUREVISION.COM

SURVEYOR MCCLURE ENGINEERING **CONTACT NAME:** ADDRESS:

STEVE WHITAKER 1700 SWIFT ST STE 100 NORTH KANSAS CITY, MO 64116 816.756.0444 EXT. 2850 SWHITAKER@MCCLUREVISION.COM



North Kansas City, Missouri 64116 P 816-756-0444 F 816-756-1763

Ankeny, IA | Carroll, IA | Clive, IA Cedar Rapids, IA | Fort Dodge, IA North Liberty, IA | Sioux City, IA Macon,MO | Columbia,MO North Kansas City,MO Lenexa, KS

NOTICE:

McClure Engineering Co. is not responsible or liable for any issues, claims, damages, or losses (collectively, "Losses") which arise from failure to follow these Plans, Specifications, and the engineering intent they convey, or for Losses which arise from failure to obtain and/or follow the engineers' or surveyors' guidance with respect to any alleged errors, omissions, inconsistences, ambiguities, or conflicts contained within the Plans and Specifications.

MISSOURI CERTIFICATE OF AUTHORITY NO. E-2006023253 EXPIRES: DECEMBER 31, 2024

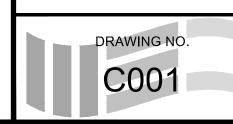
REVISIONS

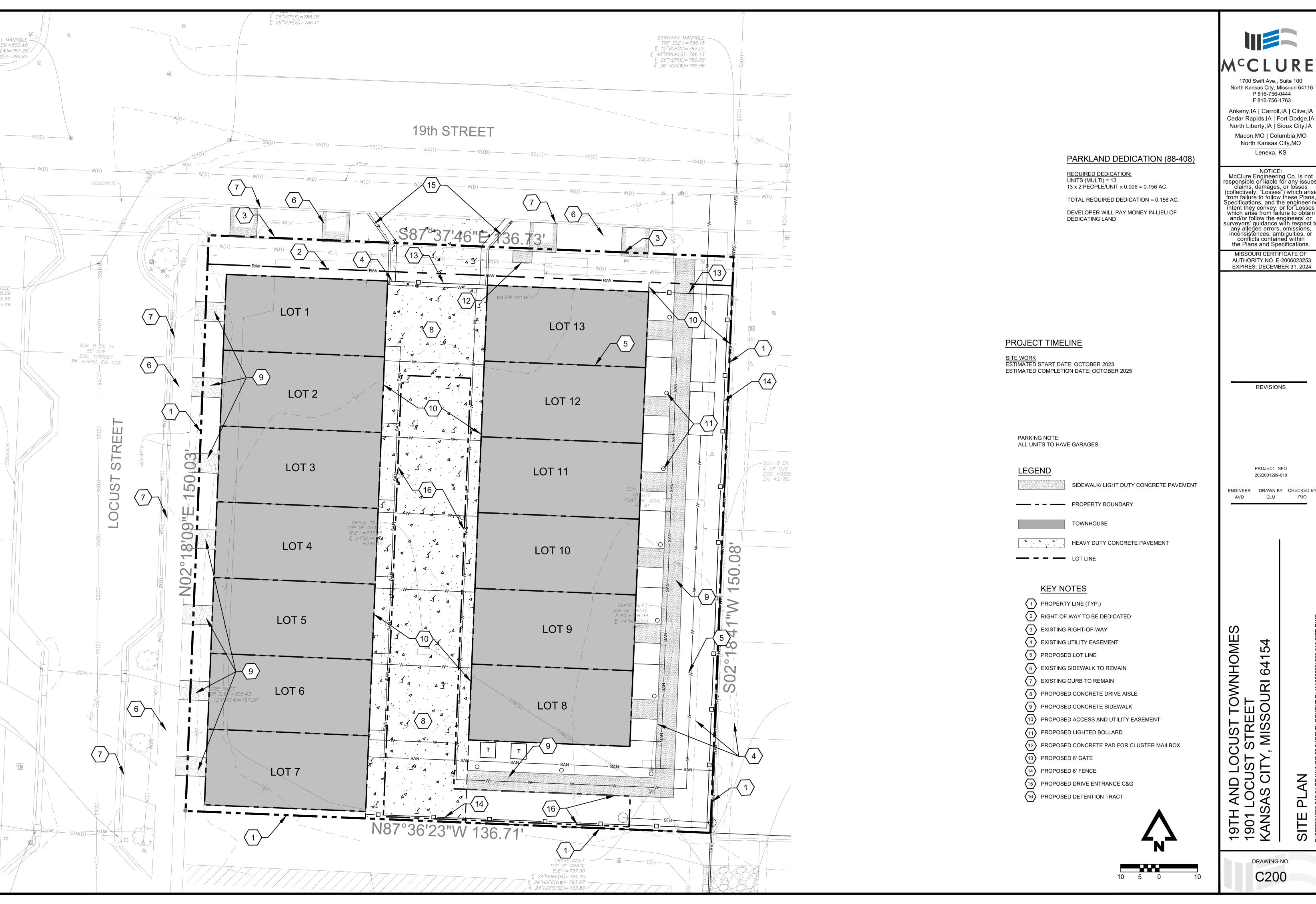
PROJECT INFO

ENGINEER DRAWN BY CHECKED BY

2022001299-010

19TH AND LOCUST TOWNHOM 1901 LOCUST STREET KANSAS CITY, MISSOURI 64154





1700 Swift Ave., Suite 100

Ankeny, IA | Carroll, IA | Clive, IA Cedar Rapids,IA | Fort Dodge,IA North Liberty, IA | Sioux City, IA Macon,MO | Columbia,MO North Kansas City,MO

NOTICE: McClure Engineering Co. is not responsible or liable for any issues, claims, damages, or losses (collectively, "Losses") which arise from failure to follow these Plans, rrom failure to follow these Plans,
Specifications, and the engineering
intent they convey, or for Losses
which arise from failure to obtain
and/or follow the engineers' or
surveyors' guidance with respect to
any alleged errors, omissions,
inconsistences, ambiguities, or
conflicts contained within
the Plans and Specifications.

MISSOURI CERTIFICATE OF AUTHORITY NO. E-2006023253 EXPIRES: DECEMBER 31, 2024

ENGINEER DRAWN BY CHECKED BY

# 19TH & LOCUST TOWNHOMES PRELIMINARY PLAT

**LOCATED IN** 

LEGAL DESCRIPTION LOT 2, 19 AND OAK, A SUBDIVISION IN KANSAS CITY, JACKSON COUNTY, MISSOURI.

	velopment Summary Table			
Proj	ject#	2022001299-010		
Proj	ject Name:	19TH AND LOCUST		
а		Zoning		
	Existing	M1-5		
	Proposed	M1-5		
b	<u> </u>	al Land Area		
~	Existing		Acres	
			Acres	
С		ht-of-Way	A	
	Existing		Acres	
	Proposed		Acres	
d		t Land Area		
	Existing		Acres	
	Proposed	0.47	Acres	
е	Prop	posed Uses		
	ТО	WNHOME		
f	Structure Heig	ht & Number of Floor	 S	
	Number of floors		stories	·
	Height Above Grade		feet	BLDG
	Number of floors		stories	·
	Height Above Grade		feet	BLDG
	Number of floors		stories	
			feet	BLDG
	Height Above Grade Number of floors		stories	
			<del> </del>	BLDG
	Height Above Grade		feet	
	Number of floors		stories	BLDG
	Height Above Grade		feet	i
	Number of floors		stories	BLDG
	Height Above Grade		feet	
	Number of floors		stories	BLDG
	Height Above Grade		feet	
	Number of floors	3	stories	BLDG
	Height Above Grade	45	feet	DED 0
	Number of floors	3	stories	BLDG
	Height Above Grade	45	feet	BLDG
	Number of floors	3	stories	BLDG
	Height Above Grade	45	feet	BLDG .
	Number of floors	3	stories	DI DC
	Height Above Grade	45	feet	BLDG
	Number of floors	3	stories	- DI D.C.
	Height Above Grade		feet	BLDG
	Number of floors		stories	
	Height Above Grade		feet	BLDG
	Gross Floor Area & Number of Ur		1000	
Б	Building Footprint		SF	
	Gross Area Per Building	· · · · · · · · · · · · · · · · · · ·	SF	
	Number of Buildings		EA	
	Total Gross Area	· · · · · · · · · · · · · · · · · · ·	SF	
h	Building Coverage & Floor Area R			
	Coverage (Total Footprint Area)	32,370		
	FAR	1.58		
i	Density			
	Gross	68,732	SF per A	cre
	Net	68,732	SF per A	cre
j	Vehicle Parking	Garage Provided		
	Ratio Required	1 Dwelling per unit		
	Stalls Provided	26		
<u> </u>	Bike Parking	Interior Spaces Provid		
•	Short-Term Ratio Required	o niterior spaces Provid	<u>acu</u>	
	·			
	Short-Term Stalls Provided	0		
	Long-Term Ratio Required	0	INITES	
	Long-Term Stalls Provided	<u> </u>	INTERIC	γ <b>К</b>
1	Timeline			
	Estimated Start Date:	10/1/2023		
	Estimated Completion Date:	10/1/2025		
	Amendments to Development PI			

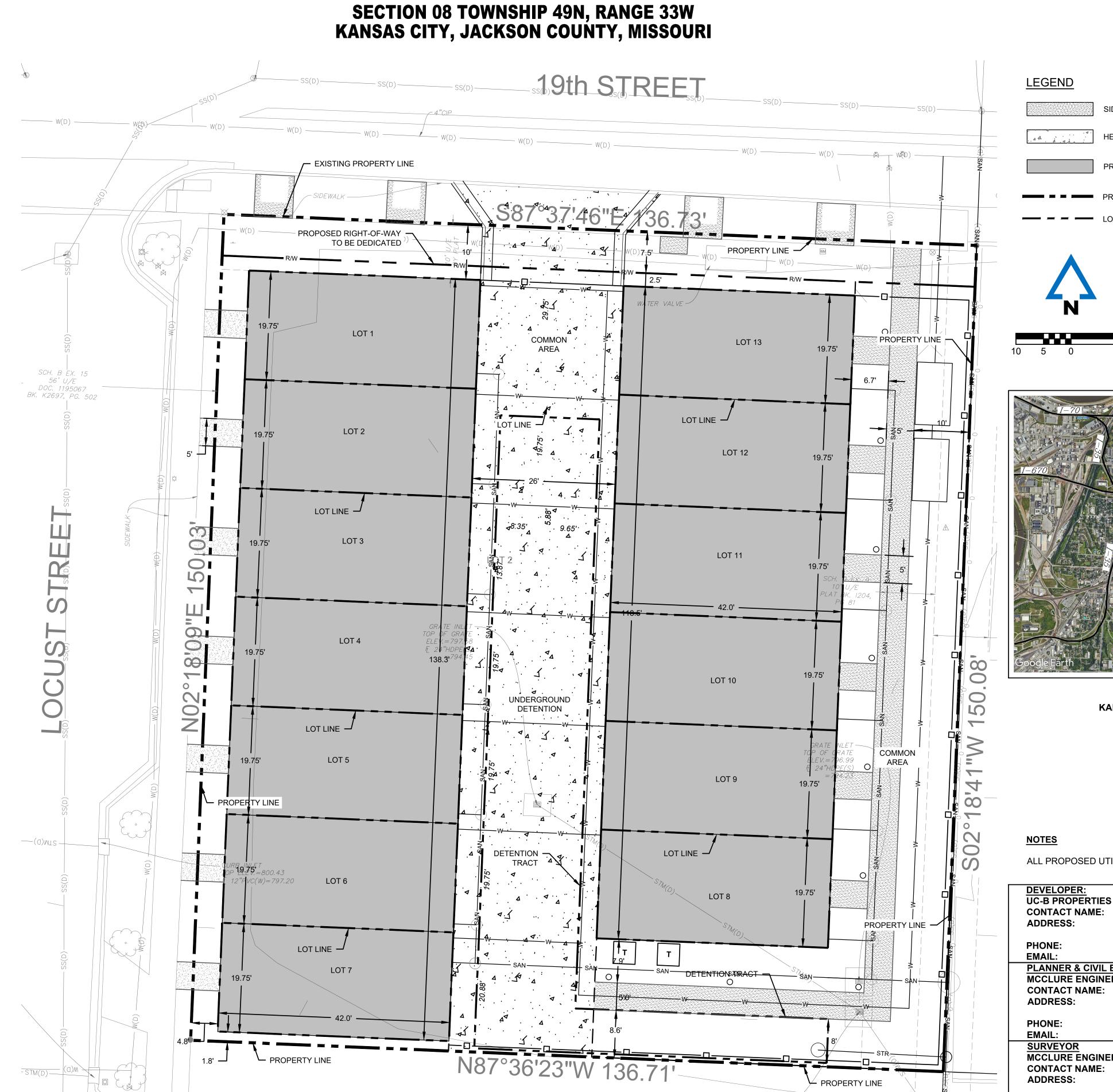
# LOT/TRACT INFO

LOT 1: 829.50 SQ.FT. OR 0.02 AC. LOT 2: 829.50 SQ.FT. OR 0.02 AC. LOT 3: 829.50 SQ.FT. OR 0.02 AC. LOT 4: 829.50 SQ.FT. OR 0.02 AC. LOT 5: 829.50 SQ.FT. OR 0.02 AC. LOT 6: 829.50 SQ.FT. OR 0.02 AC. LOT 7: 829.50 SQ.FT. OR 0.02 AC. LOT 8: 829.50 SQ.FT. OR 0.02 AC.

LOT 9: 829.50 SQ.FT. OR 0.02 AC. LOT 10: 829.50 SQ.FT. OR 0.02 AC

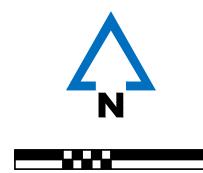
LOT 11: 829.50 SQ.FT. OR 0.02 AC. LOT 12: 829.50 SQ.FT. OR 0.02 AC.

LOT 13: 829.50 SQ.FT. OR 0.02 AC. DETENTION TRACT: 2438.00 SQ.FT. OR 0.06 AC. COMMON AREA: 6267.17 SQ.FT. OR 0.14 AC.





SIDEWALK/ LIGHT DUTY CONCRETE PAVEMENT HEAVY DUTY CONCRETE PAVEMENT PROPOSED BUILDING





LOCATION MAP NOT TO SCALE KANSAS CITY, JACKSON COUNTY, MISSOURI

PHONE:

ALL PROPOSED UTILITIES ARE PRIVATE UNLESS OTHERWISE NOTED.

**4325 TROOST AVENUE** KANSAS CITY, MO 64110 816.599.8651 **EMAIL:** LCARLTON@UC-BPROPERTIES.COM **PLANNER & CIVIL ENGINEER** MCCLURE ENGINEERING **CONTACT NAME: ADDRESS:** 

**ADAM DEGONIA** 1700 SWIFT ST STE 100 NORTH KANSAS CITY, MO 64116 816.502.3915 ADEGONIA@MCCLUREVISION.COM

LANCE CARLTON

SURVEYOR MCCLURE ENGINEERING **CONTACT NAME: ADDRESS:** 

STEVE WHITAKER 1700 SWIFT ST STE 100 NORTH KANSAS CITY, MO 64116 816.756.0444 EXT. 2850 SWHITAKER@MCCLUREVISION.COM North Kansas City, Missouri 64116

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P 816-756-0444

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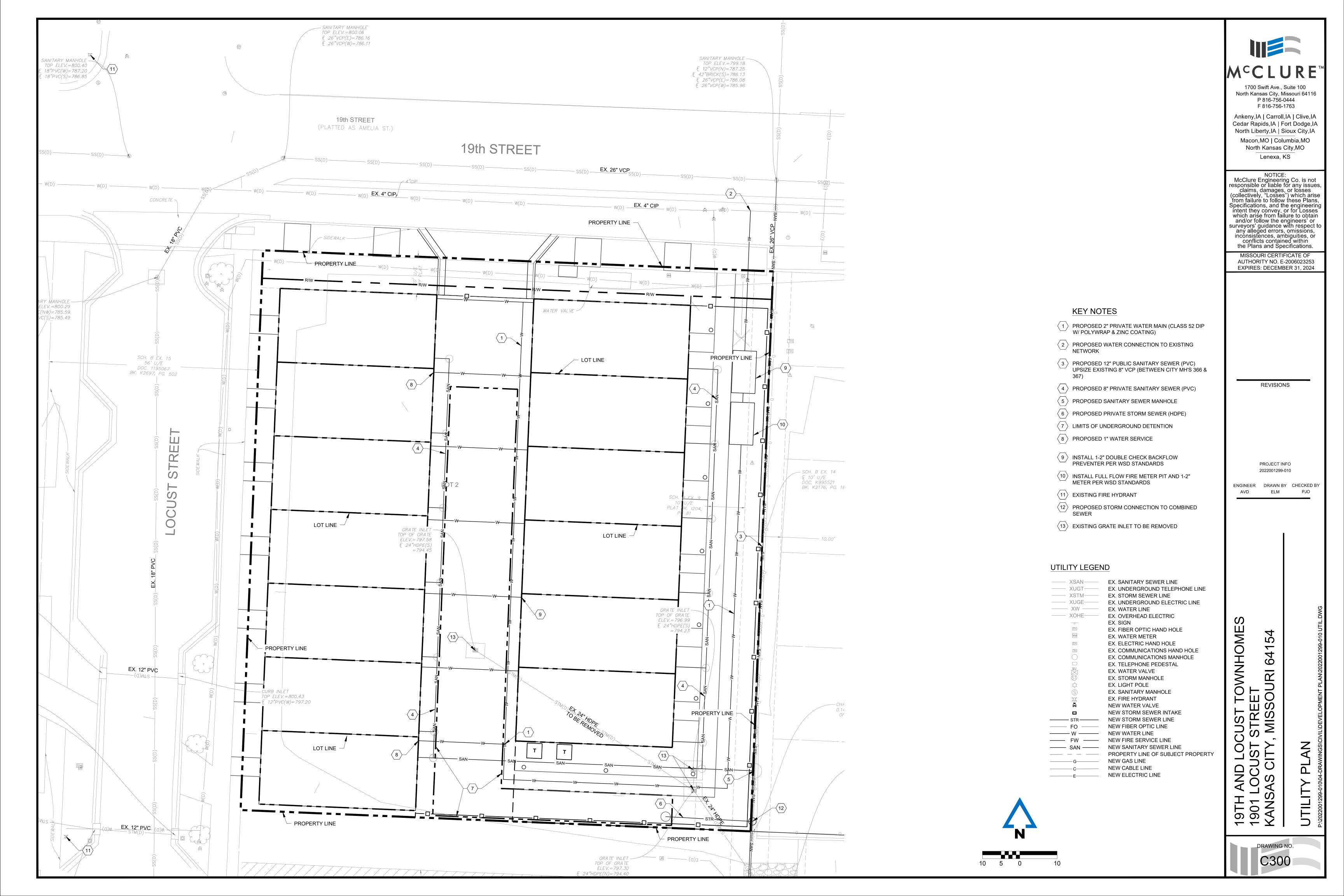
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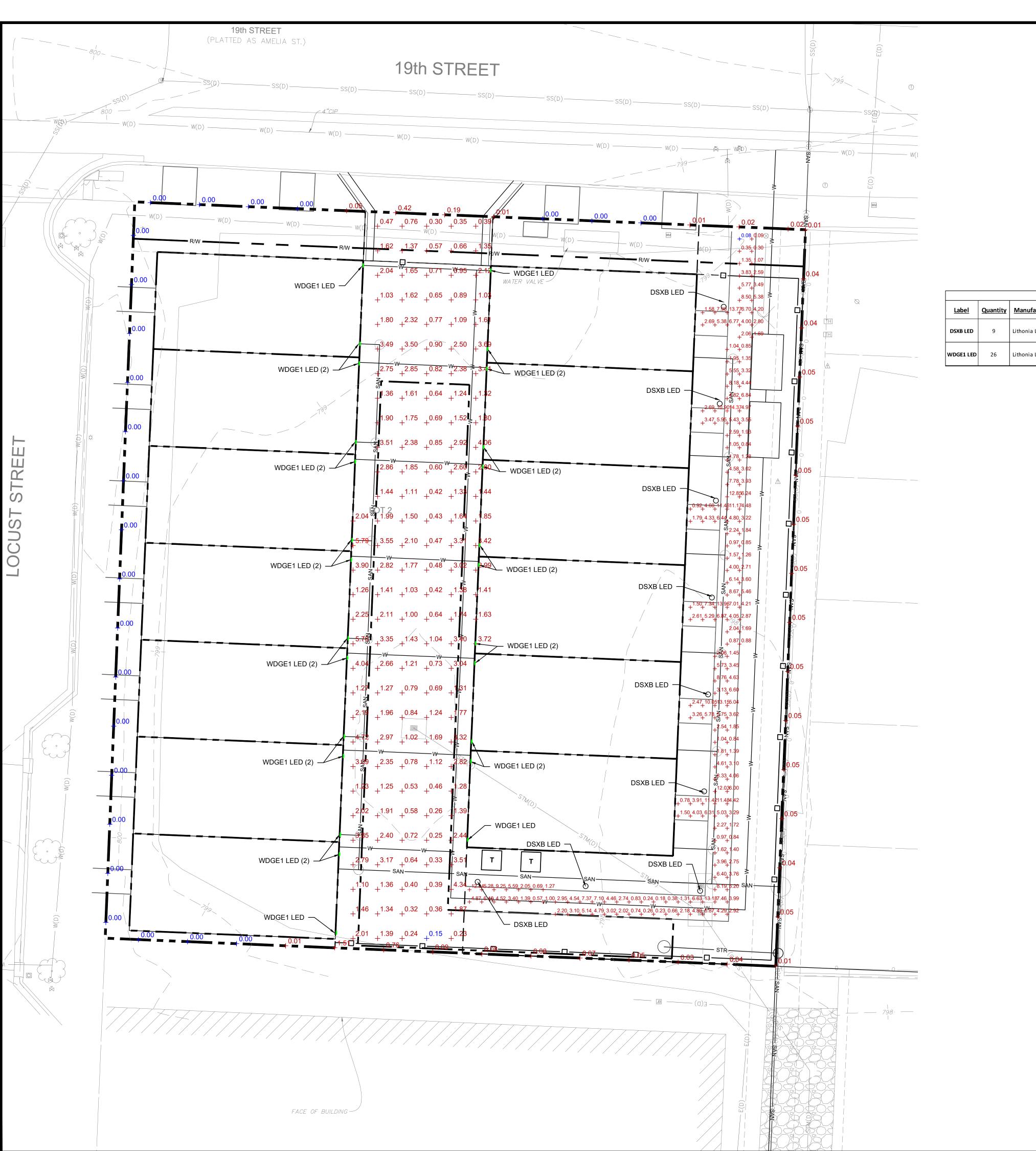
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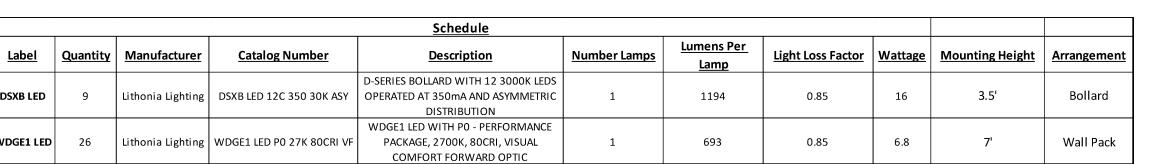
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C201

9TH 901







<u>Statistics</u>											
<u>cription</u>	Avg	<u>Max</u>	<u>Min</u>	Max/Min	Avg/Min						
e Aisle	1.75 fc	5.79 fc	0.15 fc	36.8:1	11.7:1						
erty Line	0.07 fc	1.51 fc	0.00 fc	N/A	N/A						
walk	4.20 fc	14.37 fc	0.08 fc	179.6:1	52.5:1						



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**PHOTOMETRIC** 

19TH AND LOCUST TOWNHOME 1901 LOCUST STREET KANSAS CITY, MISSOURI 64154 C301





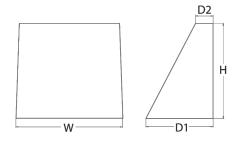
SIGNATION CHITTLE 20 BAA

**Specifications** Depth (D1): Depth (D2): Height: Width:

WDGE LED Family Overview

E4WH<sup>3</sup> Emergency battery backup, Certified in CA Title 20 MAEDBS (4W, 0°C min)

(without options)



# Introduction

The WDGE LED family is designed to meet specifier's every wall-mounted lighting need in a widely accepted shape that blends with any architecture. The clean rectilinear design comes in four sizes with lumen packages ranging from 1,200 to 25,000 lumens, providing true site-wide

WDGE1 delivers up to 2,000 lumens with a soft, non-pixelated light source, creating a visually comfortable environment. The compact size of WDGE1, with its integrated emergency battery backup option, makes it an ideal over-the-door wall-mounted lighting solution.

DDBTXD Textured dark bronze

Rev. 11/21/22

Luminaire	Standard EM, 0°C	Cold EM20°C		Lumens (4000K)							
Luminaire	Standard Ew., U.C.	Cold ENI, -20 C						P4			
WDGE1 LED	4W			1,200	2,000	)		-			
WDGE2 LED	10W	18W	Standalone / nL	ight 1,200	2,000	)	3,000	4,500	6,000		
WDGE3 LED	15W	18W	Standalone / nL	ight 7,500	8,500	)	10,000	12,000	-	-	
WDGE4 LED	-		Standalone / nL	ight 12,000	16,00	0	18,000	20,000	22,000	25,000	
Ordering Information EXAMPLE: WDGE1 LED P2 40K 80CRI VF MVOLT SRM PE DDBXD											
Series	Package Co	olor Temperature	CRI	Distribution		Voltage	Mounting				
WDGE1 LED	P2 3	27K 2700K 30K 3000K 35K 3500K 400K 4000K 50K <sup>1</sup> 5000K	80CRI 90CRI	VF Visual comfort forwa VW Visual comfort wide	rd throw	MVOLT 347 <sup>2</sup>	SRM ICW Shipped s	Shipped included SRM Surface mounting bracket			

PE⁴	Photocell, Button Type	DBLXD	Black		DBLBXD	Textured	black	
DS	Dual switching (comes with 2 drivers and 2 light engines; see page 3 for details)	DNAXD	Natural aluminum		DNATXD	Textured	natura	l aluminum
DMG	0-10V dimming wires pulled outside fixture (for use with an external control, ordered separate	y) DWHXD	White		DWHGXD	Textured v	vhite	
BCE	Bottom conduit entry for back box (PBBW). Total of 4 entry points.	DSSXD	Sandstone		DSSTXD	Textured s	andsto	ne
BAA	Buy America(n) Act Compliant							
	Accessories Ordered and shipped separately.	,			ot available in		4	I E HOT GRANGE WITH B
WDGEAWS DE	DBXD WDGE 3/8inch Architectural Wall Spacer (specify finish)				not available : , DS or PE.	with	5	Not qualified for DLC. available with E4WH.
WDGE1PBBW	V DDBXD U WDGE1 surface-mounted back box (specify finish)			3 E4WH PE or	not available DS.	with		

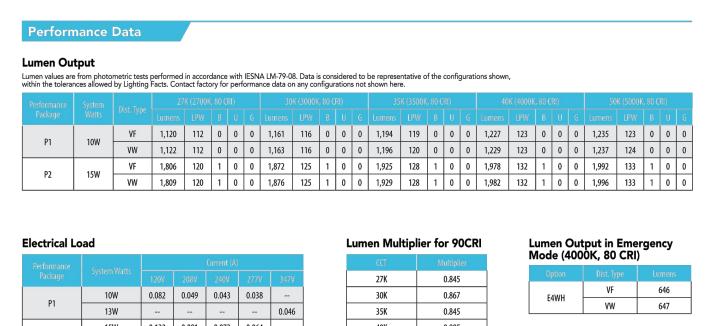
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DDBXD Dark bronze



	10W	0.082	0.049	0.043	0.038		30K	0.867		E4WH	V
P1		0.002	0.017	0.015	0.050		****			E4WH	V
	13W					0.046	35K	0.845			
P2	15W	0.132	0.081	0.072	0.064		40K	0.885			
PZ	18W					0.056	50K	0.898			
Lumen Amb	ient Tempeı	rature (	(LAT) N	/lultipli	ers		Projected LED	Lumen Maint	enance		
Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).					Data references the extrapolated performance projections for the platforms noted in ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and pro						

0°C 32°F

40°C 104°F

25°C

30°C

50°F

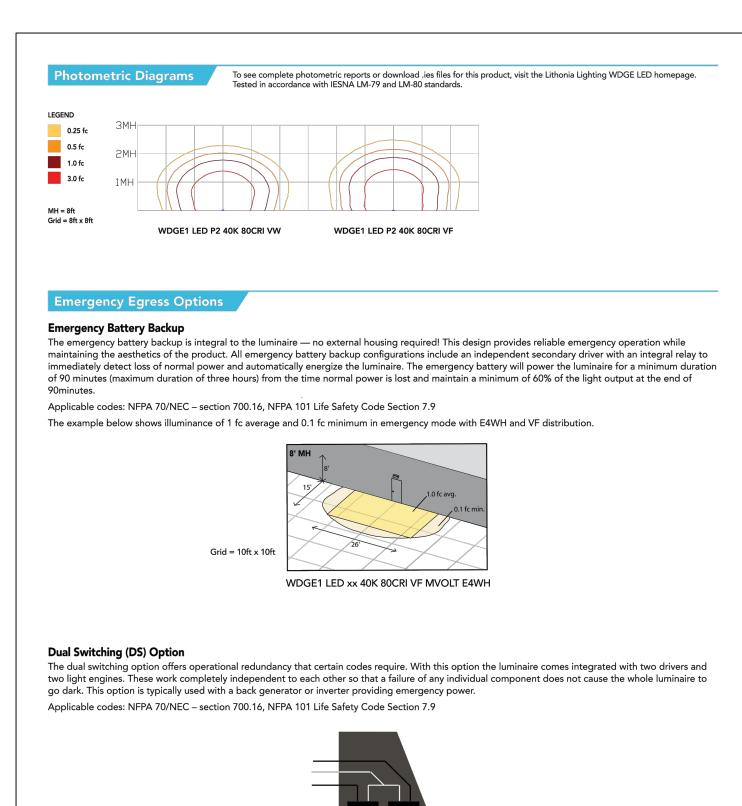
77°F

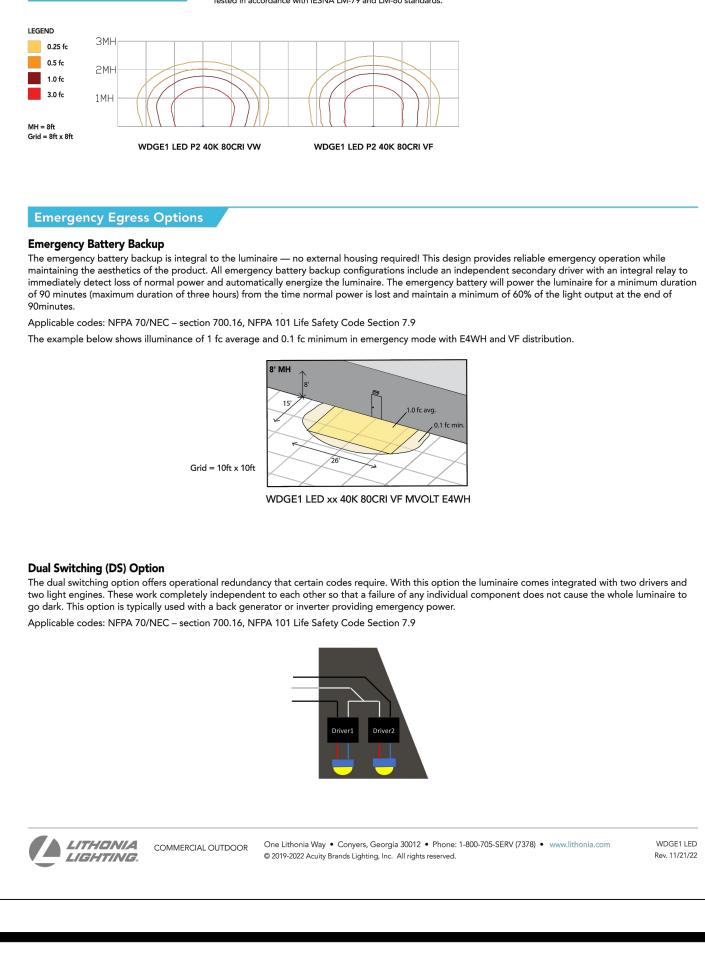
86°F

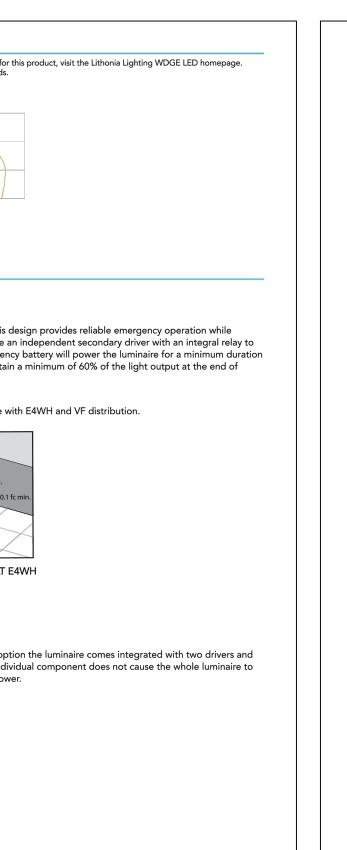
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erage ambient	i	Data references the extrapolated per ambient, based on 10,000 hours of LI IESNA TM-21-11).	formance proje ED testing (test	ctions for the placed per IESNA LI	atforms noted ir M-80-08 and pro	ı a 25°C ojected per			
Multiplier		To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.							
.03	_	g							
.05		Operating Hours	0	25,000	50,000	100,000			
.02		Operating Hours	U	23,000	30,000	100,000			
.02		Lumen Maintenance Factor	1.0	>0.96	>0.95	>0.91			
.01		Lumen Maintenance ractor	1.0	>0.90	<i>&gt;</i> 0.95	>0.91			
.01									
.00									









# Mounting, Options & Accessories FEATURES & SPECIFICATIONS CONSTRUCTION controlled multi-stage process ensures a 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling. Standard Super Durable colors include dark bronze, black, natural aluminum, sandstone and white. Available in textured and non-textured finishes. Well crafted reflector optics allow the light engine to be recessed within the luminaire, well craited renector optics allow the light engine to be recessed within the luminaire, providing visual comfort, superior distribution, uniformity, and spacing in wall-mount applications. The WDGE LED has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight. ELECTRICAL Light engine consists of high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L91/100,000 hours at 25°C). The electronic driver has a power factor of >90%, THD <20%. Luminaire comes with built in 6kV surge protection, which meets a minimum Category C low exposure (per ANSI/IEEE C62.41.2). Fixture ships standard with 0-10v dimmable driver.

Cedar Rapids, IA | Fort Dodge, IA E4WH – 4W Emergency Battery Backup PBBW - Surface-Mounted Back Box Use when there is no junction box available. D = 1.75" H = 8"W = 9" responsible or liable för any issues (collectively, "Losses") which arise from failure to follow these Plans, Specifications, and the engineering intent they convey, or for Losses which arise from failure to obtain AWS - 3/8inch Architectural Wall Space surveyors' guidance with respect to mmon architectural look, with clean rectilinear shape, of the WDGE LED was designed to A universal mounting plate with integral mounting support arms allows the fixture to hinge down for easy access while making wiring connections. The 3/8" Architectural Wall Spacer (AWS) can be used to create a floating appearance or to accommodate small imperfections in the wall surface. The ICW option can be used to mount the luminaire blend with any type of construction, whether it be tilt-up, frame or brick. Applications include commercial offices, warehouses, hospitals, schools, malls, restaurants, and other commercial inverted for indirect lighting in dry and damp locations. Design can withstand up to a 1.5 G vibration load rating per ANSI C136.31. The single-piece die-cast aluminum housing integrates secondary heat sinks to optimize thermal transfer from the internal light engine heat sinks and promote long life. The driver is mounted in direct contact with the casting for a low operating temperature and long life. CSA certified to U.S. and Canadian standards. Luminaire is IP66 rated. PIR options are The die-cast door frame is fully gasketed with a one-piece solid silicone gasket to keep out rated for wet location. Rated for -40°C minimum ambient. DesignLights Consortium® (DLC) moisture and dust, providing an IP66 rating for the luminaire. Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List Exterior painted parts are protected by a zinc-infused Super Durable TGIC thermoset at www.designlights.org/QPL to confirm which versions are qualified. International Darkpowder coat finish that provides superior resistance to corrosion and weathering. A tightly

Sky Association (IDA) Fixture Seal of Approval (FSA) is available for all products on this page utilizing 2700K and 3000K color temperature only and SRM mounting only. BUY AMERICAN ACT

Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT regulations. Please refer to www.acuitybrands.com/buy-american for additional information.

5-year limited warranty. This is the only warranty provided and no other statements in this specification sheet create any warranty of any kind. All other express and implied warranties are disclaimed. Complete warranty terms located at:

**Note:** Actual performance may differ as a result of end-user environment and application. All values are design or typical values, measured under laboratory conditions at  $25\,^{\circ}\text{C}$ . Specifications subject to change without notice.

D = 5.5"

H = 8"

W = 9"

D = 0.38"

W = 7.5"

H = 4.4"

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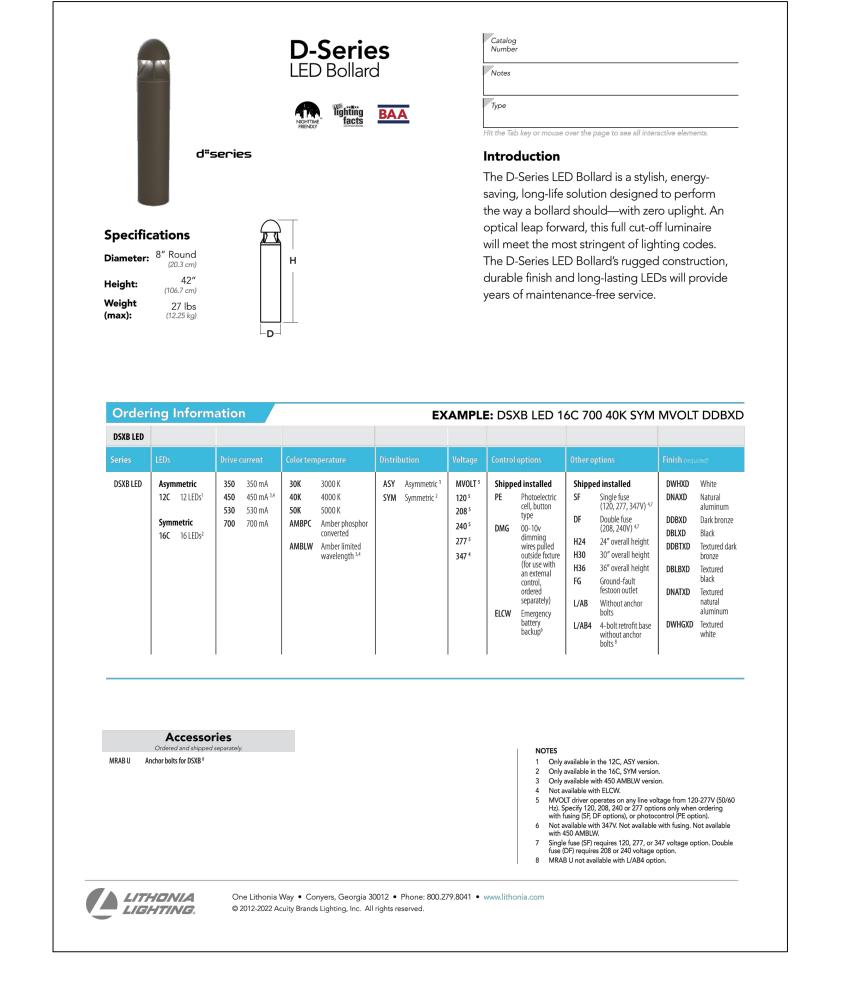
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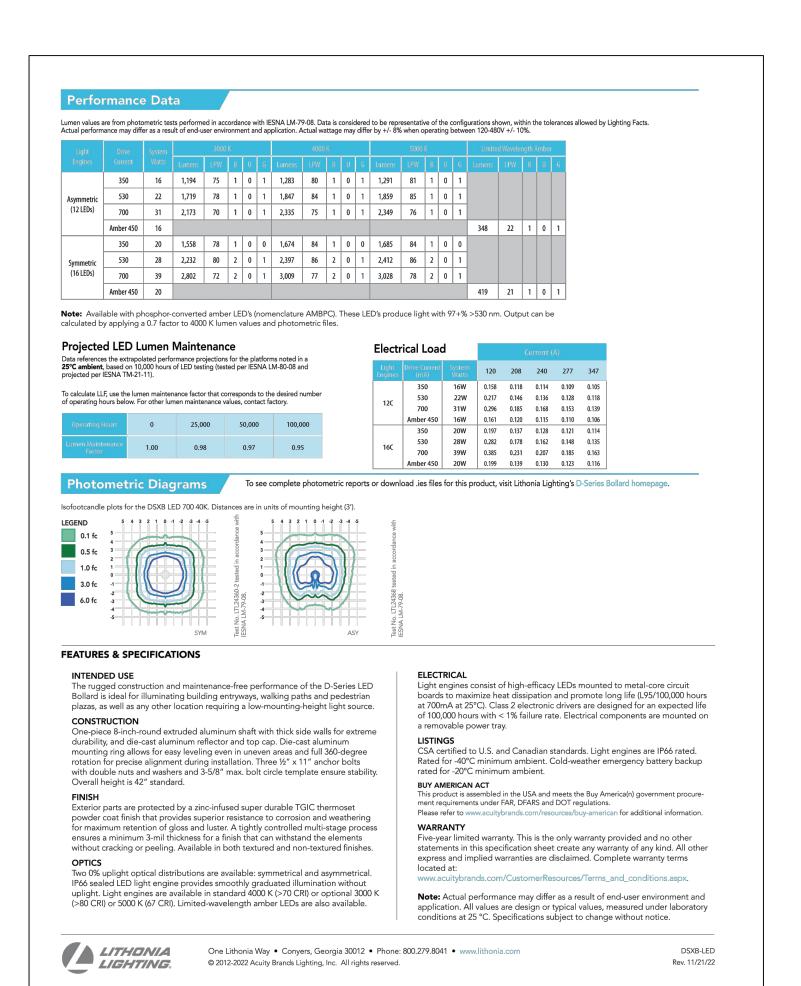
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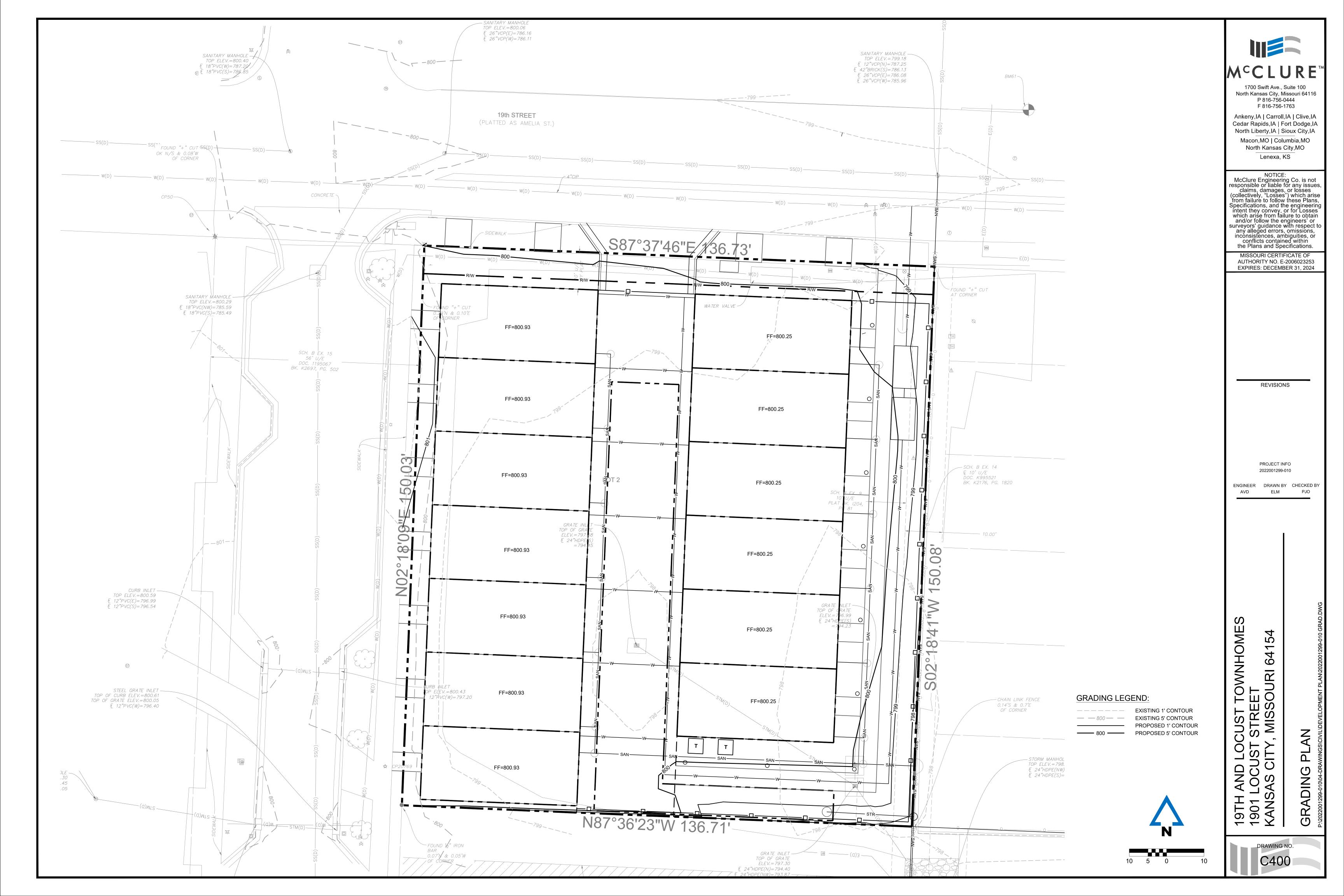
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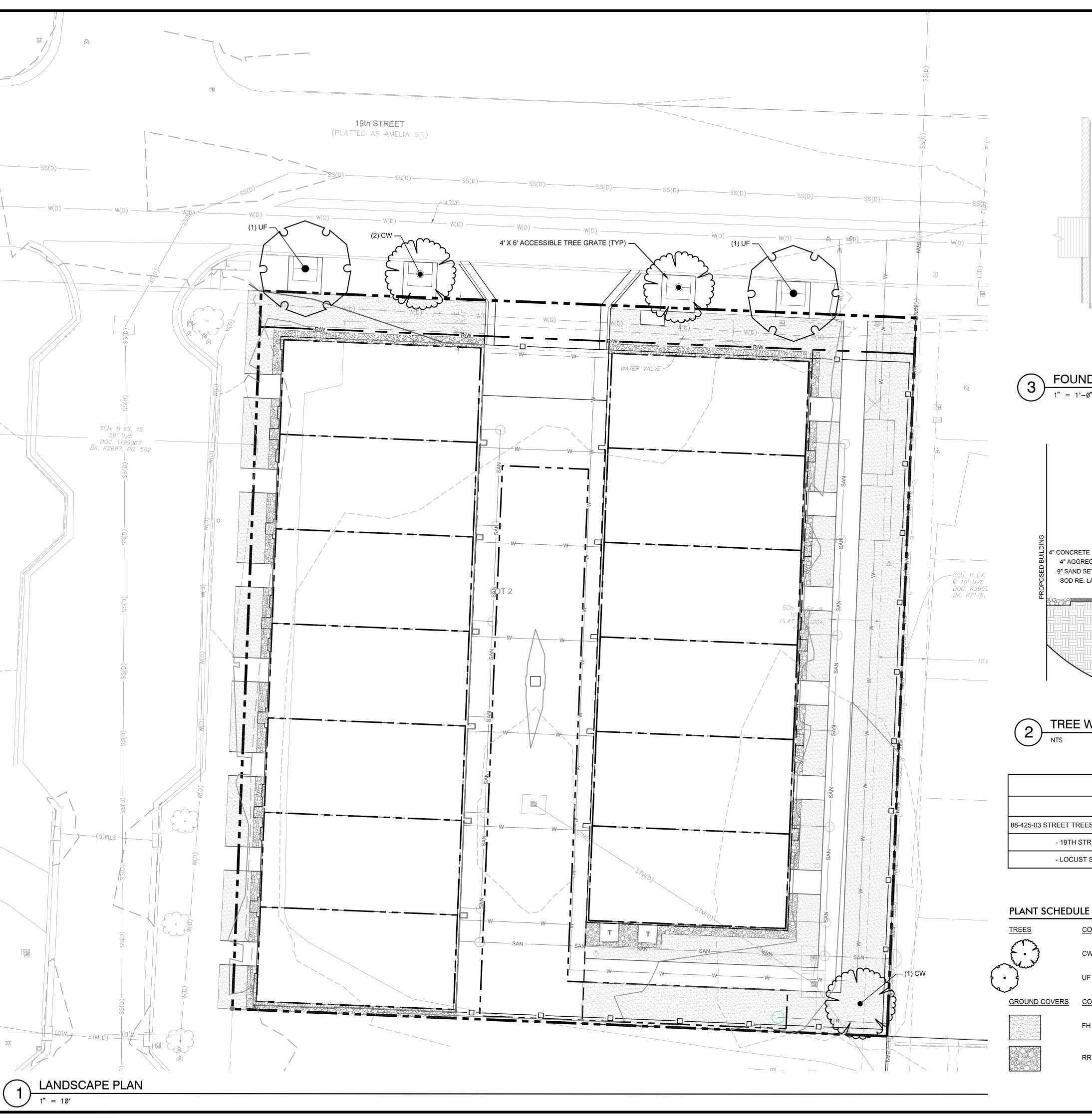
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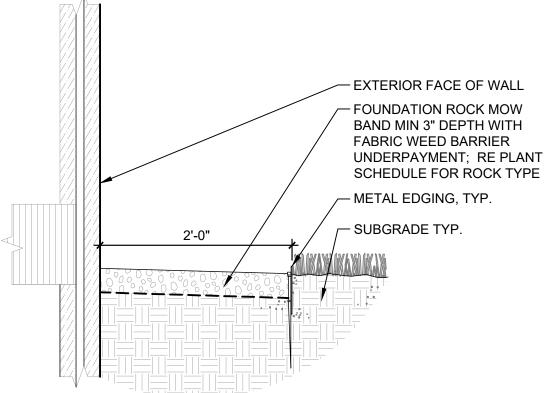
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FOUNDATION ROCK MOW BAND

329413.01-09

— 4' x 6' TREE GRATE 4" CONCRETE SIDEWALK — — PLANTING SOIL MIX 4" AGGREGATE BASE -— EXISTING CURB AND GUTTER 9" SAND SETTING BED -COMPACTED SUBGRADE SOD RE: LANDSCAPE -

TREE WELL DETAIL

LANDSCAPE REQUIREMENTS								
	REQUIREMENT	PROPOSED						
88-425-03 STREET TREES	ONE TREE REQUIRED PER 30LF OF STREET FRONTAGE							
- 19TH STREET	136 LF OF STREET FRONTAGE   4.5 TREES REQUIRED	4 TREES PROVIDED						
- LOCUST STREET	150 LF OF STREET FRONTAGE   5 TREES REQUIRED	4 STREET TREES TO REMAIN						

	TEANT SCILED	LANT SCHEDOLL				
	TREES	CODE	<u>QTY</u>	BOTANICAL / COMMON NAME	CONT	
800		CW	3	Cornus florida 'White' / White Flowering Dogwood	B&B, 2" Cal.	
		UF	2	Ulmus carpinifolia x parvifolia 'Frontier' / Frontier Elm	B&B, 2" Cal.	
	GROUND COVERS	CODE	<u>QTY</u>	BOTANICAL / COMMON NAME	CONT	
_		FH	3,911 sf	Fescue Sod / Heal-Tolerant Fescue Sod	SF	
		RR	742 sf	Rock Mulch / Kansas Native Rock	SF	



L101

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19TH AND LOCUST TOWNHOME 1901 LOCUST STREET KANSAS CITY, MISSOURI 64154

HJM ARCHITECTS

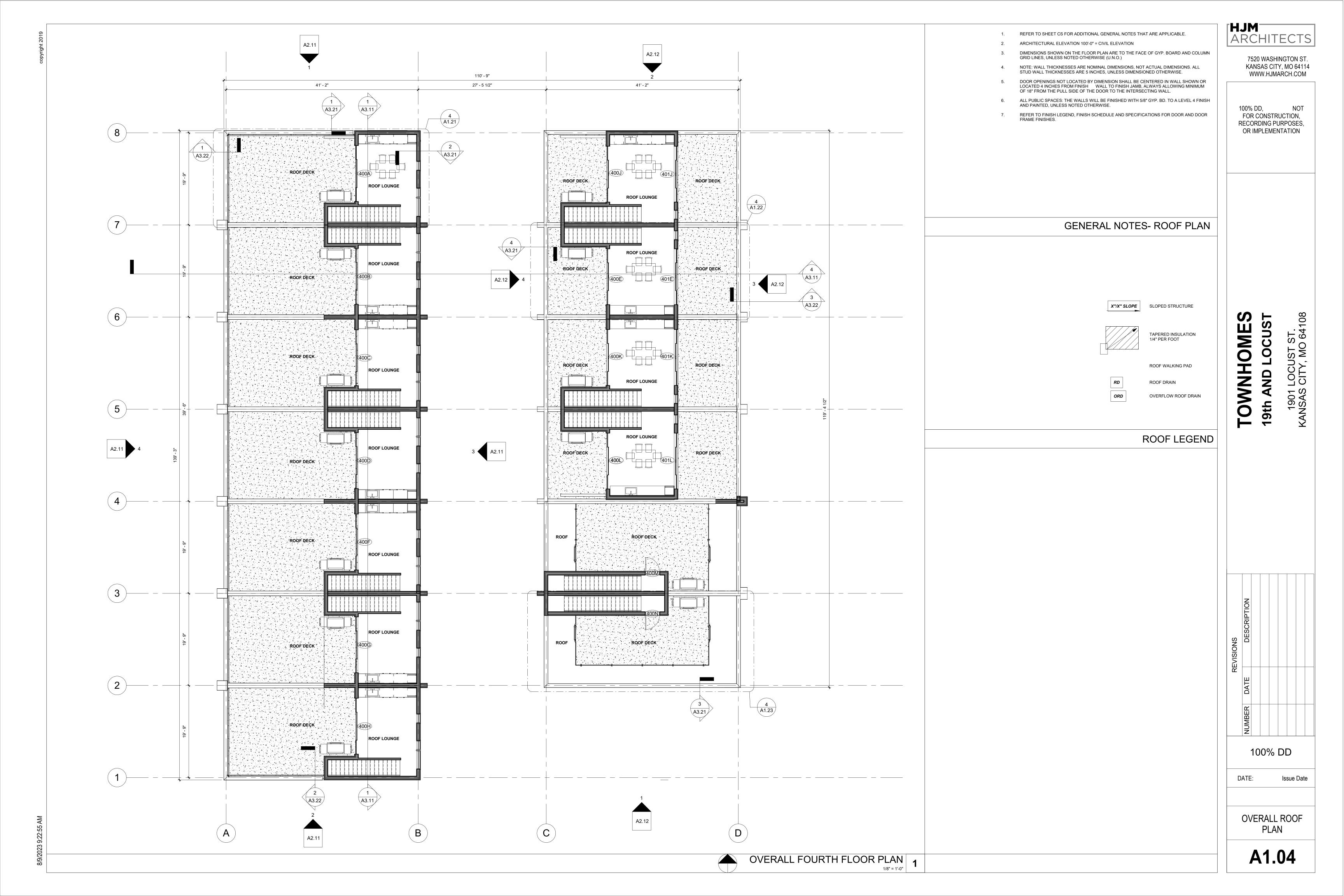
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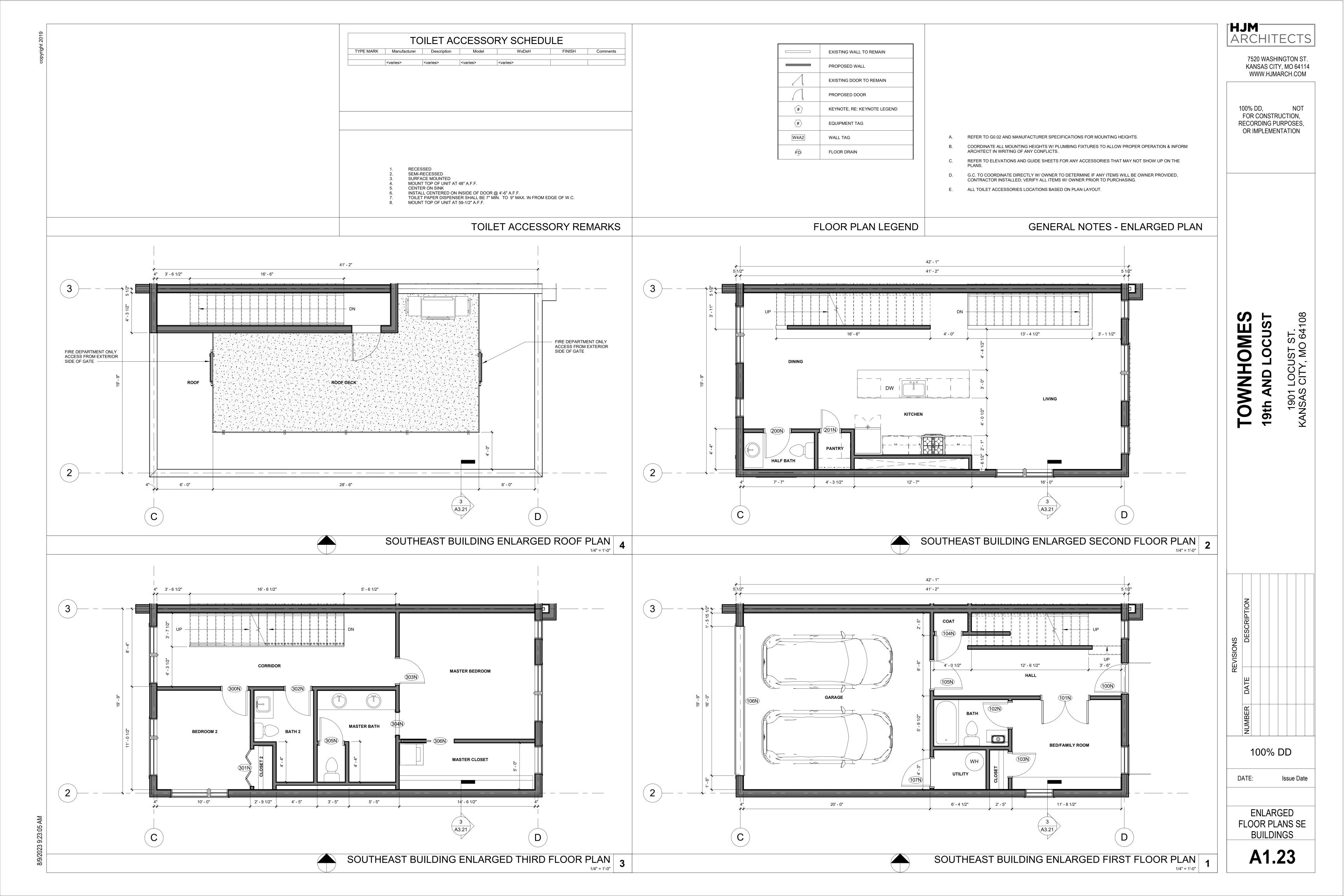
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100% DD

EXTERIOR RENDERINGS

A0.10











# CCA Infrastructure Committee Meeting - Monday, July 24th, 12PM via Google Meet

Julie Johnson <julie\_c@kccrossroads.org>

Tue, Jul 25, 2023 at 3:43 PM

To: Erica Hartley <ehartley@uc-bproperties.com>

Cc: Lance Carlton <a href="cc:com">cc: Lance Carlton@uc-bproperties.com</a>, Leland Berman <a href="cc:ceebaileyberman@gmail.com">cc: Lance Carlton@cc: Lance Carlton@uc-bproperties.com</a>, Leland Berman <a href="cc:ceebaileyberman@gmail.com">cc: Lance Carlton@cc: Lance Car

Hi All,

Thank you so much for your wonderful presentation yesterday! As you could tell, you've hit the nail on the head for what people want in the Crossroads.

I have attached the meeting minutes which include the list of attendees.

Please let me know if I may assist you in any way moving forward.

Kindest regards,

Julie

[Quoted text hidden]

**072423\_CCA Infrastructure Meeting Minutes.pdf** 4154K

# **07.24.23 CCA Infrastructure Committee Meeting Minutes**

Attendees:

Michael Sharma-Crawford, Debbie Aron, Jim Brown, Erik Wullschleger, Jan Marcason, Shirley Saffold, David Johnson, Anna Munguia, Morgan Pemberton, Jon Trozzolo, Lance Carlton, Robert Harris, Valissa Jones, Jacob Bowyer, Alex Proffer, Lee Berman, Erica Hartley, Jim Martin, Consuelo Cruz, Mark Spence, David Lopez, Amanuel Tegegne, Jenny Wheat, Lindsay Clawson, STRETCH, Vince Bryant, Leslie Tomlinson.

**Location:** Via Conference Call

<u>Called to order</u>: 12:00pm

# 1. Introductions

- 2. <u>1901 Locust Street Development</u> Lance Carlton, President of UC-B Properties & Lee Berman, Principal at Berman Co
  - Developers currently own the parcel of land. Half-acre vacant lot.
  - Specialize in small urban infill sites.
  - Site to have 13 townhomes for sale. Seven units to face Locust, 6 will face east. Access will be through 19<sup>th</sup> Street.
  - Townhomes differ from condos in that the owner owns the ground below and sky above, not only the residential unit. Townhome developers do not have to pre-sale units as condo developers must. However, both have HOA for common areas with rules & guidelines.
  - Elevation 4 story [comparable to surrounding structures]. First two stories to be brick in order to blend in with the surrounding neighborhood. Top two stories possibly stucco. "Brick building with contemporary component".
  - The façade facing 19<sup>th</sup> Street will have larger windows, additional brick.
  - <u>Sidewalk along 19th</u> will be improved while keeping integrity of existing bike lane.
    - \*Opportunity to work with Matt Abbott for improvement of the full southern sidewalk along 19<sup>th</sup> from Locust to Cherry. Current sidewalk is in extremely bad shape. Developers agreed to reach out Matt.
  - Floor Plans Approx 2,300 sq ft each [not including garage or deck]
    - 1st Level double garage, rear loaded [won't be seen from street], foyer, flexible space – home office, additional room.
    - o 2<sup>nd</sup> Level DR, LR, Kitchen.
    - o 3<sup>rd</sup> Level Master BR + 1 BR
    - o 4<sup>th</sup> Level 250-300 sq ft. entertainment room and deck.
  - No abatements were asked for on project.
  - Streetscaping? Will work with CCA Crossroads Tree Initiative.
  - Pricing? Still working on pricing but approximately in the \$800K range. Difficult finding comps for financial requirements.
  - Public or Private Trash? Private trash collection. 95 gal carts instead of dumpster.
  - Completion Date? Twelve [12] months from breaking ground. Hope to break ground this fall
  - Southern border? 15' from Milhaus project. No setbacks required.
  - Current plans attached. May have future minor variations due to city, utility and/or KCFD

requirements.

# 3. <u>Crossroads CID Update</u> – David Johnson

- Final Petition received and signature phase has begun!
- CID information can be found on the Crossroads website: https://kccrossroads.org/community-improvement-district/
- CID boundary includes the entire Crossroads neighborhood with the exclusion of both existing and currently proposed CIDs as city policy is against stacking.
- Map on website Numbers listed on parcels are for internal reference only. Your tax parcel number can be found on your tax statement or on the KC Parcel Viewer: <a href="https://maps.kcmo.org/apps/parcelviewer/">https://maps.kcmo.org/apps/parcelviewer/</a>.
- Summary of Services Reviewed: Greenspace, Security, Trash, Graffiti Removal, First Friday.
- Property Assessment based on flat rate, not property value. Will have a 3% escalation for inflation.
- Sales Tax Component ½ cent sales tax. Voted on only by registered Crossroads
   Community residents. Key component in keeping assessments affordable. For the
   majority, tax paid by visitors to the community.
- Budget 10-year budget. No great variations as services are predictable.
- Board Nine-member volunteer Board all of who are Crossroads property and business owners.
- Concern:
  - David Lopez, owner of Manny's, had concern over sales tax component as rate is already high, but appreciated the affordable flat rate on properties.
  - David also had concerns over First Friday, especially Food Truck proximity to his business.
    - CCA has worked to consolidate Food Trucks the Food Truck Plaza at 20<sup>th</sup> & Grand.
    - Businesses still have a right to host Food Trucks on their property for the evening by invitation.
    - Distance rules for Food Trucks parking on street near restaurants per City Code:
      - Street vendors shall not sell, hawk or peddle, or offer to sell, hawk or peddle any service or item within 50 feet of a public entrance of an established business offering similar products to the public during the hours that the business is open to the public. For purposes of this article, one cannot sell, hawk or peddle food items within 50 feet of a restaurant, deli, cafeteria or other eating establishment selling food items during the time it is open for business.

